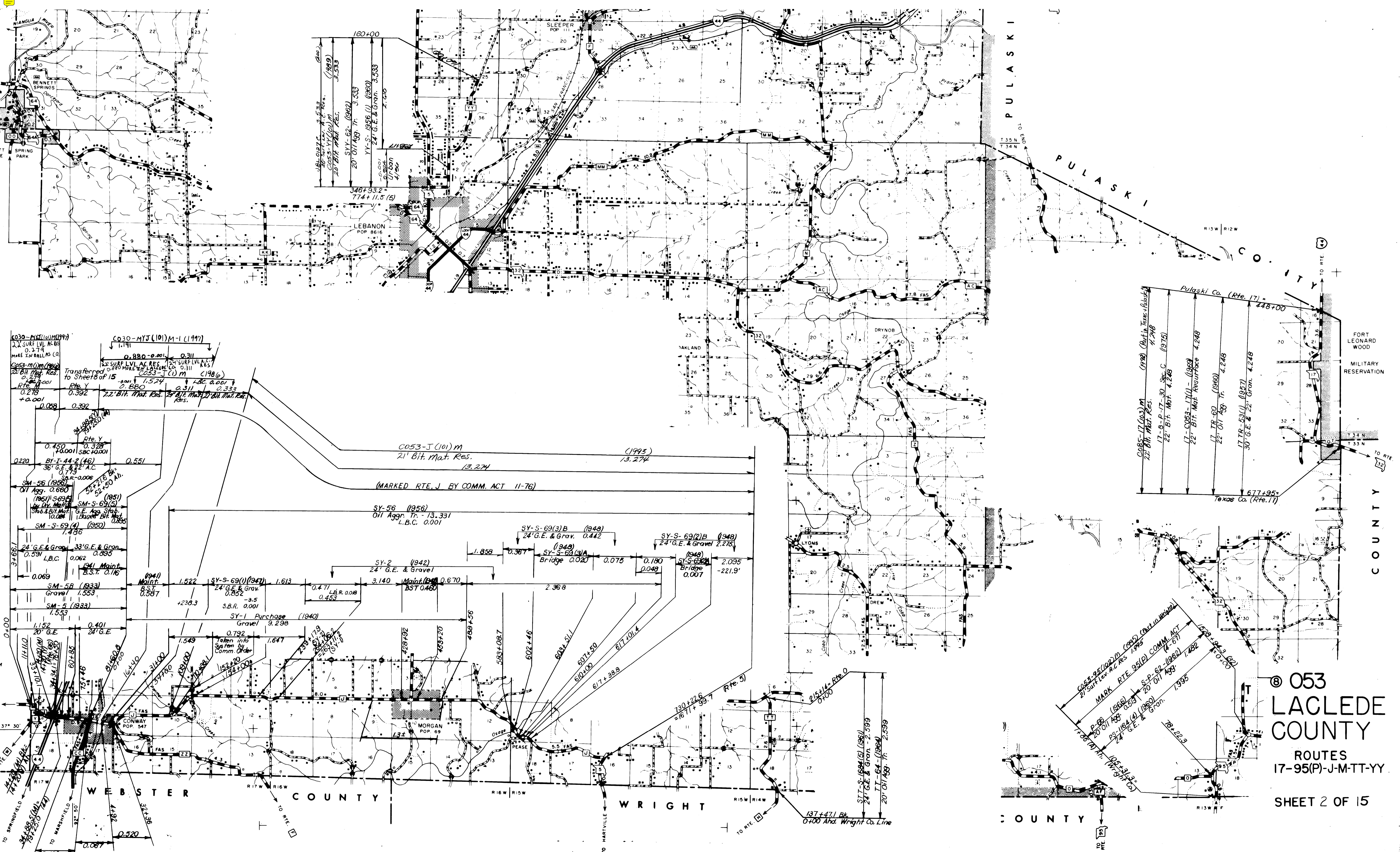


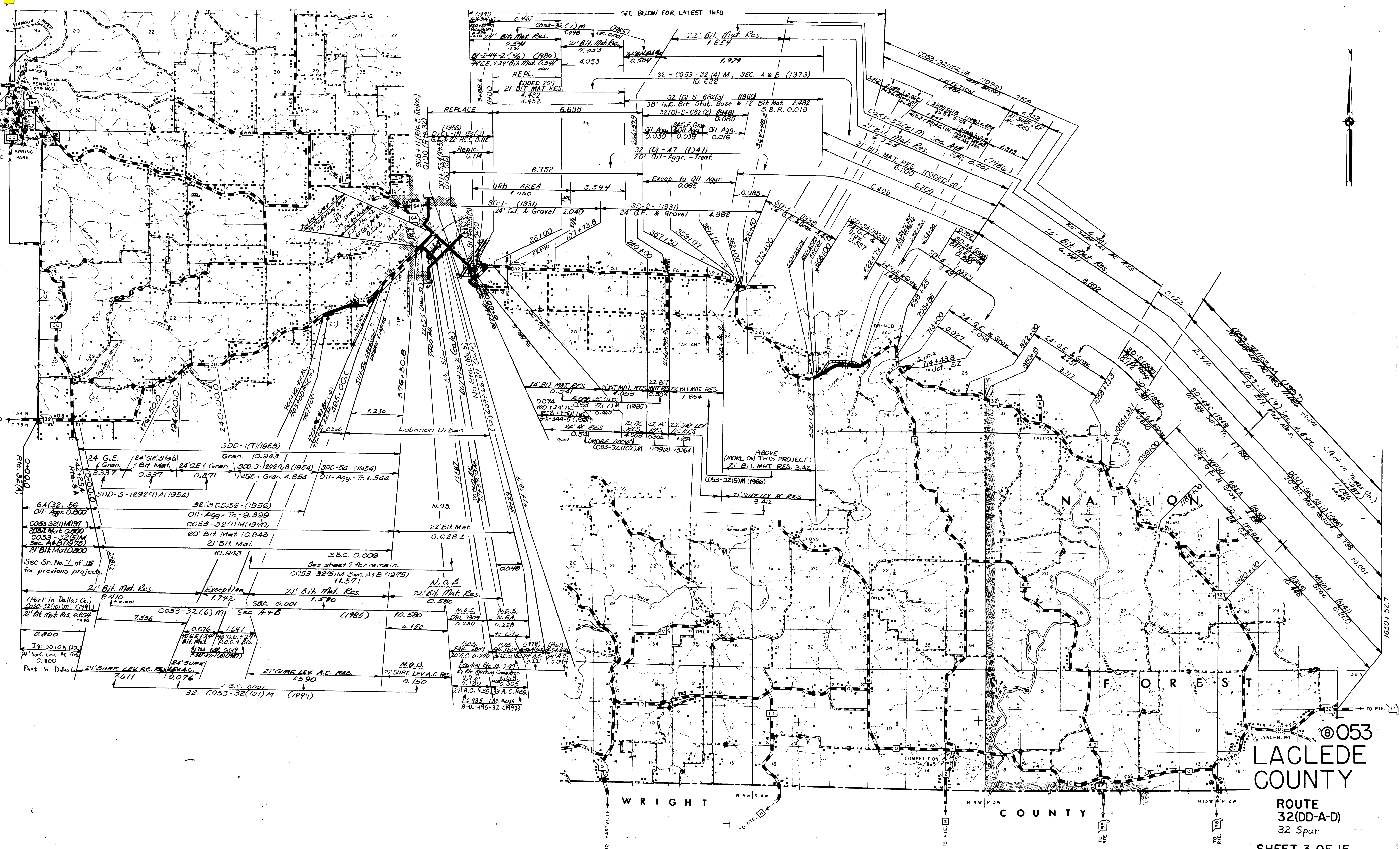
© 053
 LACLEDE
 COUNTY
 ROUTE
 5



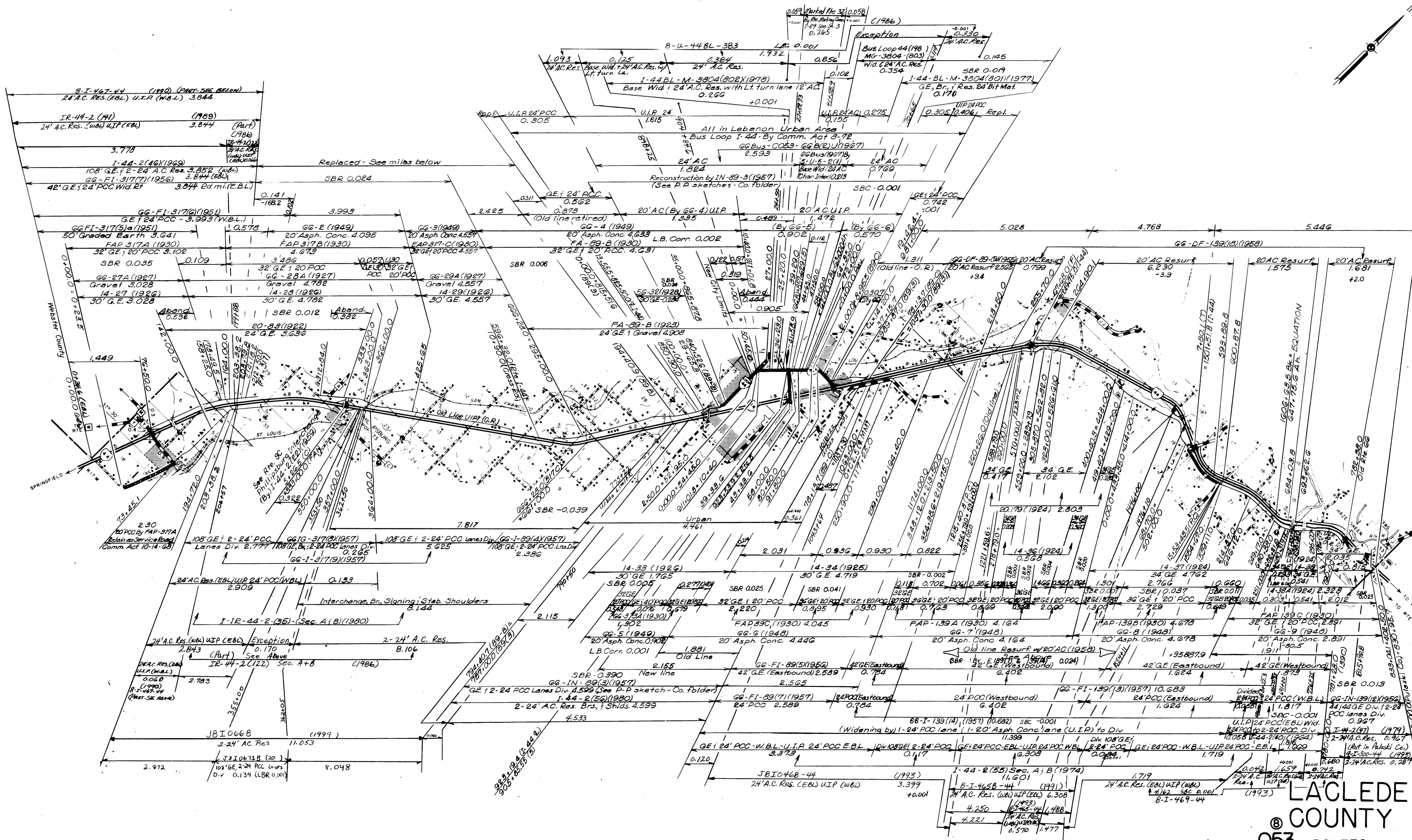
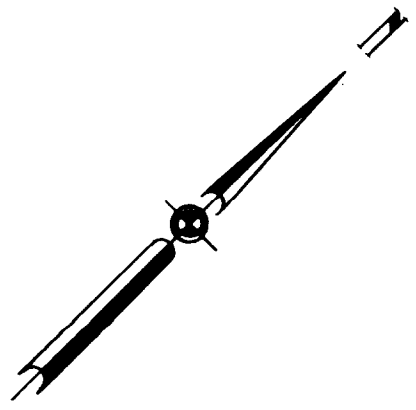
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 LACLEDE
 COUNTY

ROUTES
 17-95(P)-J-M-TT-YY

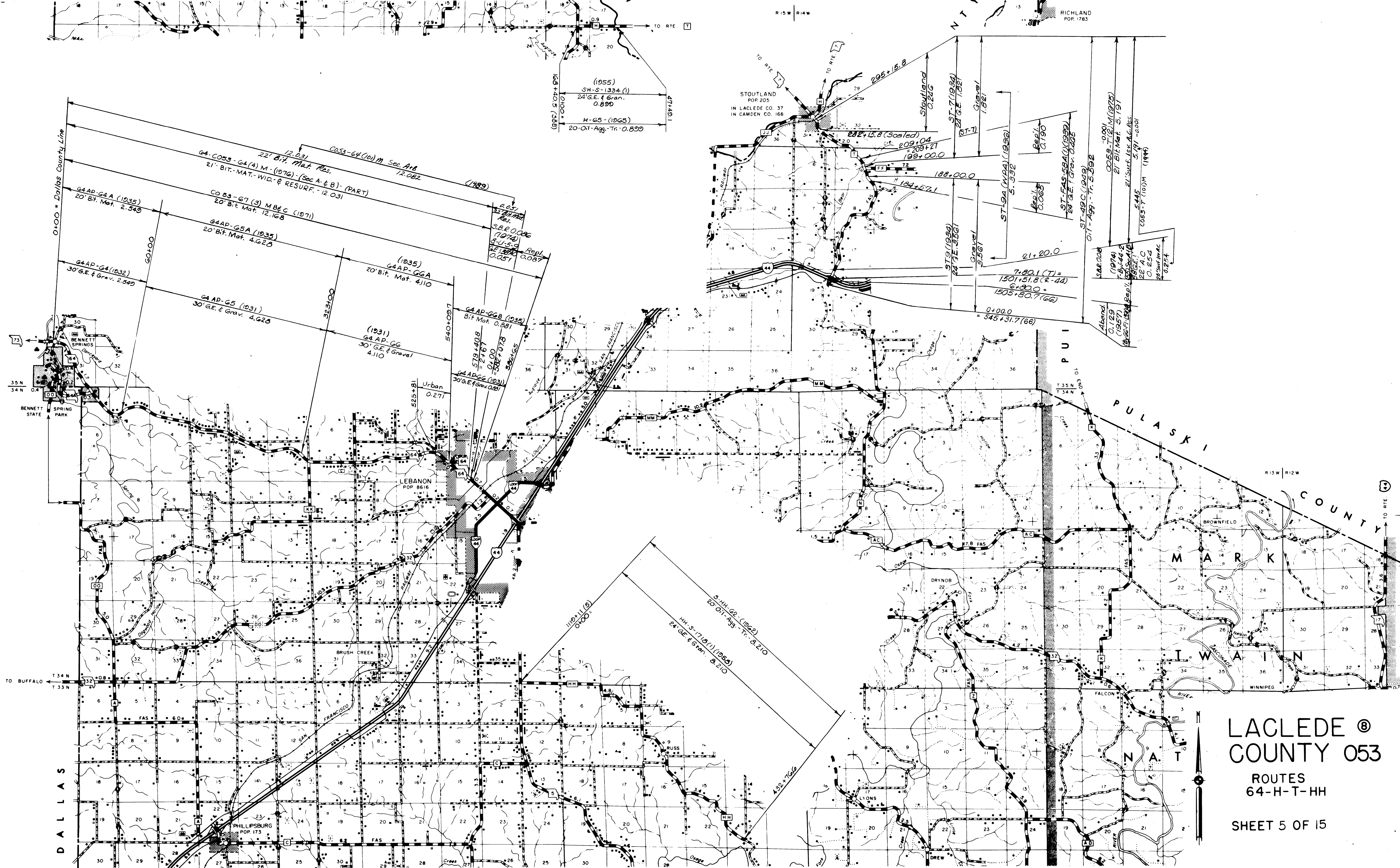
SHEET 2 OF 15



053
LACLEDE
COUNTY
 ROUTE
 32(DD-A-D)
 32 Spur
 SHEET 3 OF 15



LACLEDE
 COUNTY
 ©
 053 ROUTES
 I-44 & I-44 BUS. LOOP
 SHEET 4 OF 15



DALLAS

**LACLEDE [®]
COUNTY 053**

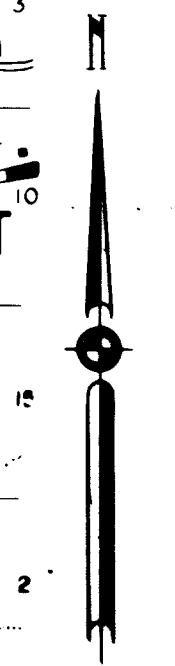
ROUTES
64-H-T-HH

SHEET 5 OF 15

RICHLAND
POP. 1783

STOUTLAND
POP. 205
IN LACLEDE CO. 37
IN CAMDEN CO. 166

PULASKI COUNTY



(1955)
SH-S-1334 (1)
24' G.E. & Gravel
0.899
H-65 (1965)
20' Oil-Agg.-Tr. 0.899

ST-7 (1932)
22' G.E. 1.821
Gravel 1.821
ST-9A (WPA) (1936)
5.332
ST-9B (1936)
24' G.E. 1.821
ST-9C (1949)
0.11-Agg.-Tr. 5.592
ST-9D (1959)
24' G.E. 1.821
ST-9E (1959)
24' G.E. 1.821
ST-9F (1959)
24' G.E. 1.821
ST-9G (1959)
24' G.E. 1.821
ST-9H (1959)
24' G.E. 1.821
ST-9I (1959)
24' G.E. 1.821

0+00 = Dallas County Line
G4 AP-G4A (1935)
20' Bit. Mat. 2.549
G4 AP-G4B (1932)
30' G.E. & Gravel 2.549
G4 AP-G4C (1931)
30' G.E. & Gravel 4.420
G4 AP-G4D (1931)
30' G.E. & Gravel 4.410
G4 AP-G4E (1931)
30' G.E. & Gravel 4.410
G4 AP-G4F (1931)
30' G.E. & Gravel 4.410
G4 AP-G4G (1931)
30' G.E. & Gravel 4.410
G4 AP-G4H (1931)
30' G.E. & Gravel 4.410
G4 AP-G4I (1931)
30' G.E. & Gravel 4.410
G4 AP-G4J (1931)
30' G.E. & Gravel 4.410
G4 AP-G4K (1931)
30' G.E. & Gravel 4.410
G4 AP-G4L (1931)
30' G.E. & Gravel 4.410
G4 AP-G4M (1931)
30' G.E. & Gravel 4.410
G4 AP-G4N (1931)
30' G.E. & Gravel 4.410
G4 AP-G4O (1931)
30' G.E. & Gravel 4.410
G4 AP-G4P (1931)
30' G.E. & Gravel 4.410
G4 AP-G4Q (1931)
30' G.E. & Gravel 4.410
G4 AP-G4R (1931)
30' G.E. & Gravel 4.410
G4 AP-G4S (1931)
30' G.E. & Gravel 4.410
G4 AP-G4T (1931)
30' G.E. & Gravel 4.410
G4 AP-G4U (1931)
30' G.E. & Gravel 4.410
G4 AP-G4V (1931)
30' G.E. & Gravel 4.410
G4 AP-G4W (1931)
30' G.E. & Gravel 4.410
G4 AP-G4X (1931)
30' G.E. & Gravel 4.410
G4 AP-G4Y (1931)
30' G.E. & Gravel 4.410
G4 AP-G4Z (1931)
30' G.E. & Gravel 4.410

HH-S-1710 (1) (1958)
24' G.E. & Gravel 8.210
S-HH-G2 (1962)
20' Oil-Agg.-Tr. 8.210

TO BUFFALO

TO RTE. 1

TO RTE. 3

TO RTE. 4

TO RTE. 5

TO RTE. 6

TO RTE. 7

TO RTE. 8

TO RTE. 9

TO RTE. 10

TO RTE. 11

TO RTE. 12

TO RTE. 13

TO RTE. 14

TO RTE. 15

TO RTE. 16

TO RTE. 17

TO RTE. 18

TO RTE. 19

TO RTE. 20

TO RTE. 21

TO RTE. 22

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TO RTE. 24

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TO RTE. 28

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TO RTE. 41

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TO RTE. 43

TO RTE. 44

TO RTE. 45

TO RTE. 46

TO RTE. 47

TO RTE. 48

TO RTE. 49

TO RTE. 50

TO RTE. 51

TO RTE. 52

TO RTE. 53

TO RTE. 54

TO RTE. 55

TO RTE. 56

TO RTE. 57

TO RTE. 58

TO RTE. 59

TO RTE. 60

TO RTE. 61

TO RTE. 62

TO RTE. 63

TO RTE. 64

TO RTE. 65

TO RTE. 66

TO RTE. 67

TO RTE. 68

TO RTE. 69

TO RTE. 70

TO RTE. 71

TO RTE. 72

TO RTE. 73

TO RTE. 74

TO RTE. 75

TO RTE. 76

TO RTE. 77

TO RTE. 78

TO RTE. 79

TO RTE. 80

TO RTE. 81

TO RTE. 82

TO RTE. 83

TO RTE. 84

TO RTE. 85

TO RTE. 86

TO RTE. 87

TO RTE. 88

TO RTE. 89

TO RTE. 90

TO RTE. 91

TO RTE. 92

TO RTE. 93

TO RTE. 94

TO RTE. 95

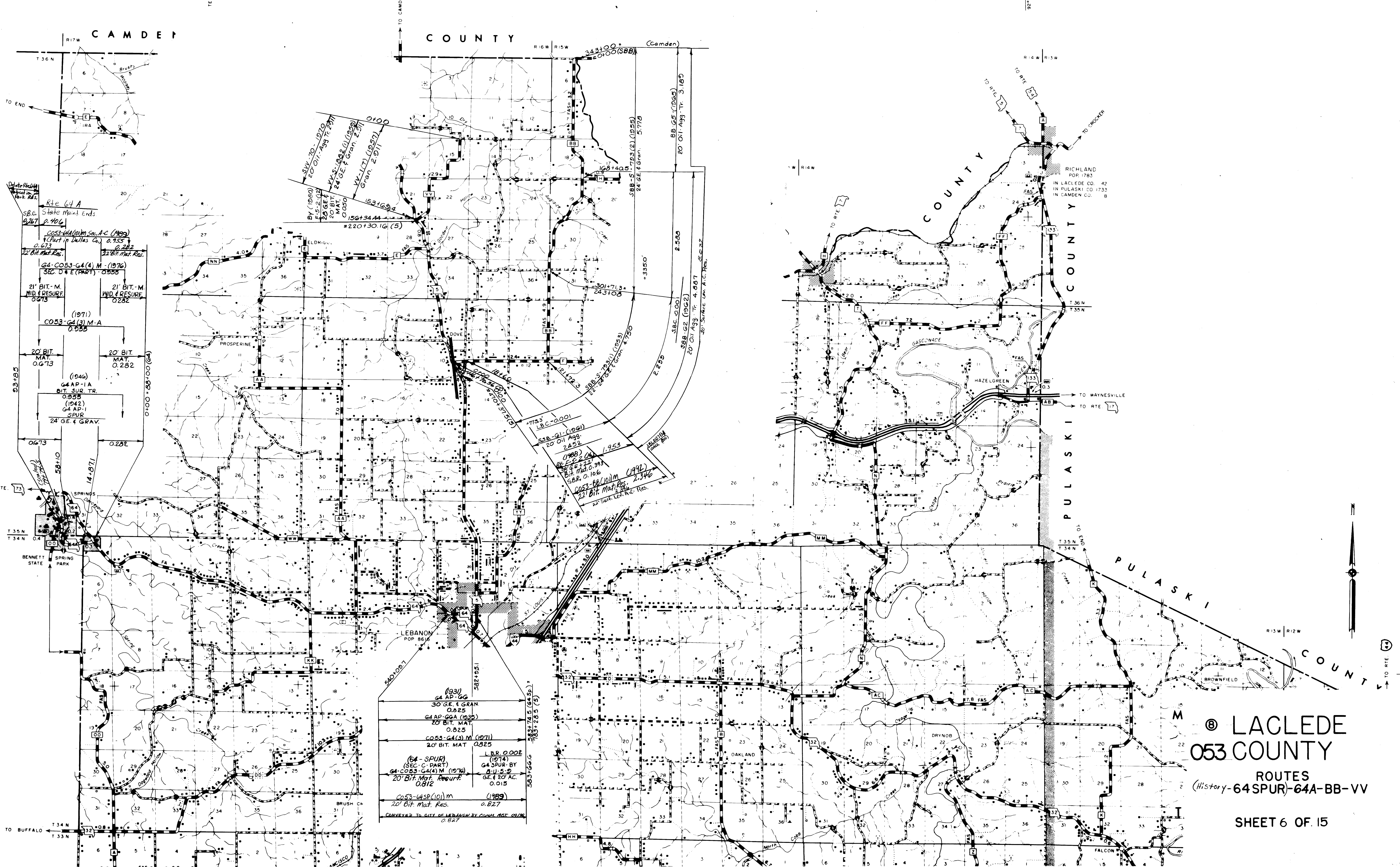
TO RTE. 96

TO RTE. 97

TO RTE. 98

TO RTE. 99

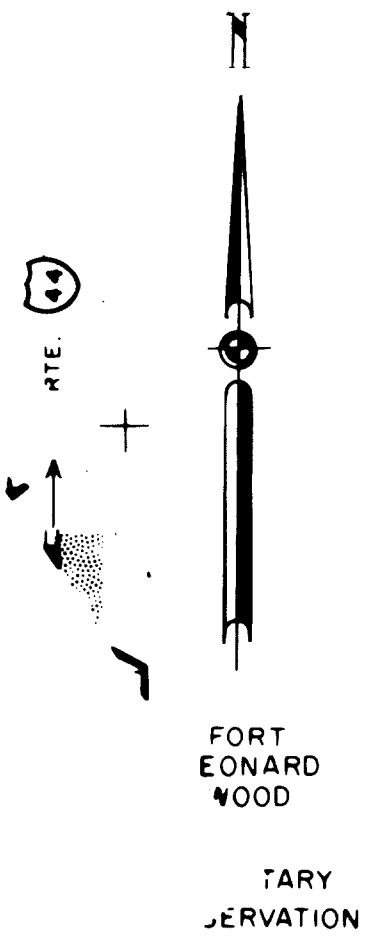
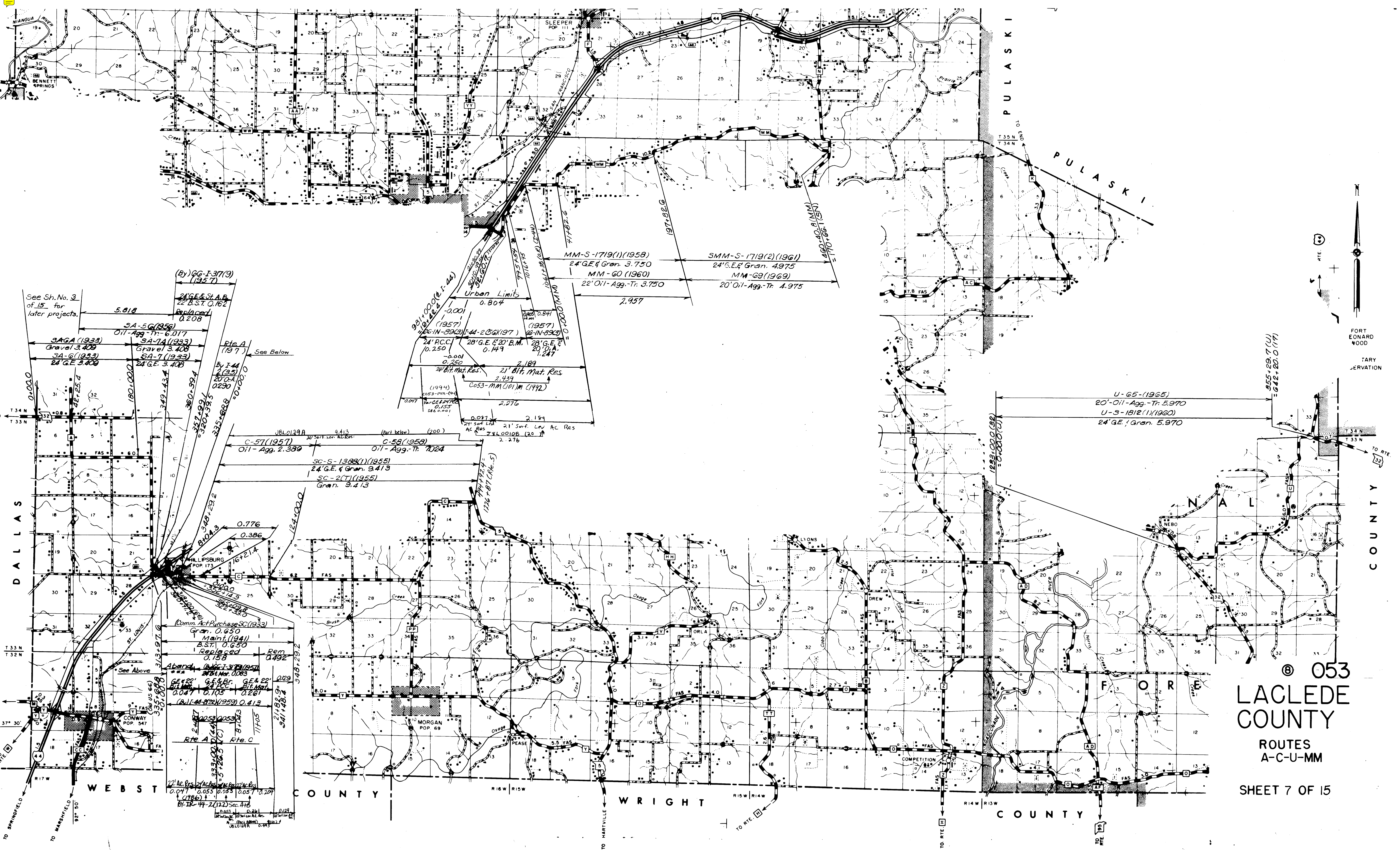
TO RTE. 100



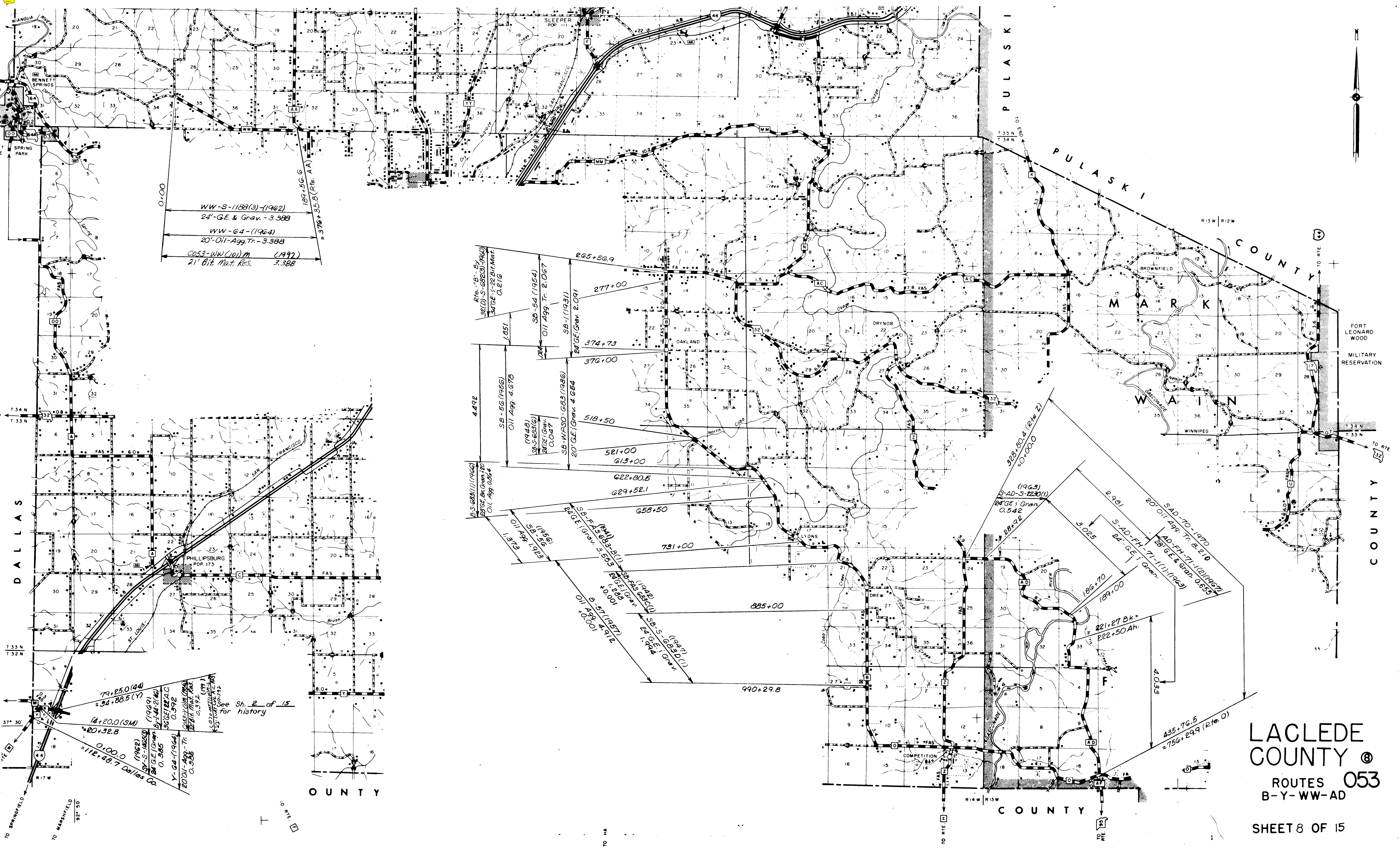
Deleted Rte 64
 Rte 64 A
 State Maint Ends
 S.B.C. 0.267
 22' Bit. Mat. Res. 0.282
 G4-CO53-G4(4) M (1976)
 SEC D & E (PART) 0.555
 21' BIT. M. WID. & RESURE 0.282
 (1971)
 CO53-G4(3) M-A 0.555
 20' BIT. MAT. 0.673
 20' BIT. MAT. 0.282
 (1946)
 G4 AP-1 A BIT. SUR. TR. 0.925
 (1942)
 G4 AP-1 SPUR 24 GE. & GRAV. 0.100 + 88' 100 (64)

(1931)
 G4 AP-GG
 30 GE. & GRAN 0.825
 G4 AP-GA (1935)
 20' BIT. MAT. 0.825
 CO53-G4(3) M (1971)
 20' BIT. MAT. 0.825
 (64-SPUR)
 (SEC-C PART)
 G4-CO53-G4(4) M (1976)
 20' BIT. Mat. Resurf. 0.812
 CO53-G4SP(10) M (1989)
 20' Bit. Mat. Res. 0.827
 CONVEYED TO CITY OF LEBANON BY COM. ACT. 01/18/87

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 053 COUNTY
 ROUTES
 (History-64 SPUR)-64A-BB-VV
 SHEET 6 OF 15



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LACLEDE COUNTY
 ROUTES
 A-C-U-MM
 SHEET 7 OF 15



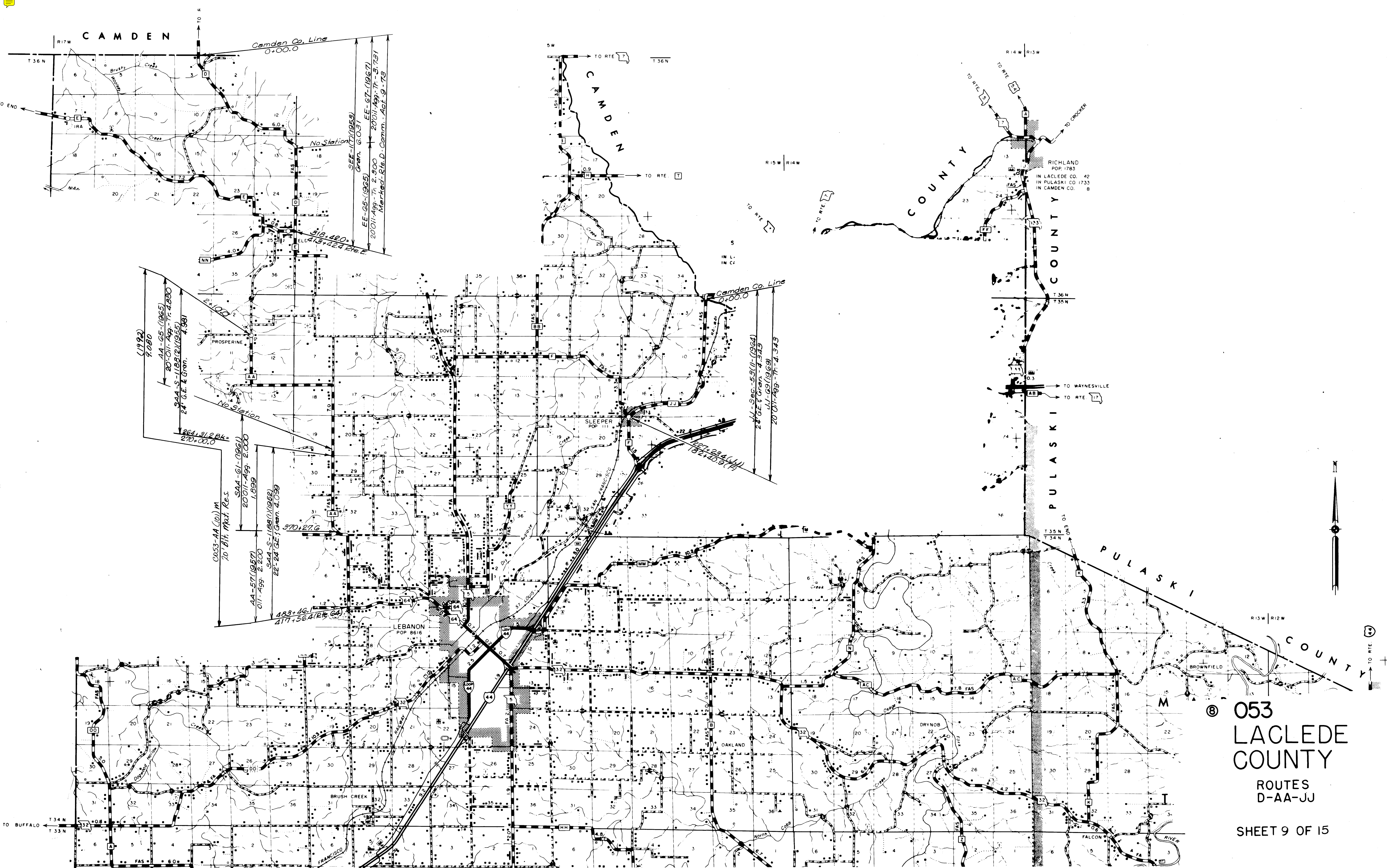
WW-S-1188(3)-(1962)
 24'-GE. & Grav. - 3.388
 WW-64-(1964)
 20'-Oil-Agg. Tr. - 3.388
 C053-WW(10)M (1992)
 21' Bit. Mat. Res. 3.388

79+25.0 (44)
 = 34,88.5 (Y)
 14+20.0 (SM)
 = 20+32.8
 0+00.0
 = 112+49.7 Dallas Co.
 (1962)
 SY-S-1425(3)
 24' GE. Grav. 0.388
 Y-64-(1964)
 20' Oil-Agg. Tr. 0.388
 C053-WW(10)M (1992)
 21' Bit. Mat. Res. 0.388
 C053-WW(10)M (1992)
 21' Bit. Mat. Res. 0.388
 See Sh. 2 of 15
 for history

189+56.6
 = 376+35.8 (Rte. AA)
 265+56.9
 277+00
 374+73
 376+00
 518+50
 521+00
 613+00
 622+80.5
 629+52.1
 658+50
 731+00
 885+00
 990+29.8
 186+70
 189+00
 221+27 Bk.
 = 222+50 Ah.
 435+76.5
 = 756+29.9 (Rte. O)

(1948)
 SB-S-683(6)
 24' GE. Grav. 0.047
 SB-WP50-G63 (1936)
 20' GE. Grav. 4.654
 SB-54 (1954)
 Oil-Agg. Tr. 2.067
 SB-1 (1931)
 24' GE. Grav. 2.091
 SB-FAS-683-B(1)
 24' GE. Grav. 5.553
 SB-FAS-683-C(1)
 24' GE. Grav. 1.288
 SB-S-683-D(1)
 24' GE. Grav. 1.954
 SB-51 (1957)
 Oil-Agg. Tr. 4.912
 SB-AD-S-2230(1)
 24' GE. Grav. 0.542
 S-AD-FH-71-1(1)-(1969)
 24' GE. Grav. 2.981
 S-AD-FH-71-1(2)-(1962)
 24' GE. Grav. 3.025
 S-AD-70-1970
 20' Oil-Agg. Tr. B. 2.10
 AD-FH-71-1(2)-(1962)
 24' GE. Grav. 0.655

LACLEDE COUNTY ®
 ROUTES 053
 B-Y-WW-AD
 SHEET 8 OF 15



CAMDEN

Camden Co. Line
0+00.0

SEE-177(1953)
Grn. 6.031
EE-65-(1965)
20011-App. Tr. 2.300
Marked R.E.D. Comm. - Act. 9-73

No Station

518.440
413+424 Pk.E

(1992)
9.080
AA-65-(1965)
20-011-App. Tr. 4.280
SAA-S-1188(1955)
24 G.E. & Grn. 4.981

264.312 Bk.
270+00.0

0053-AA (100) M
20' Bk. Mat. Res.

SAA-61-(1961)
20011-App. 2.000
1.899

AA-57(1957)
011-App. 2.200

SAA-S-1188(1955)
22-24 G.E. & Grn. 4.059

483+40.1
417+56.4 Bk. (CA)

Camden Co. Line
0+00.0

LL-96-53(1)-(1964)
24 G.E. & Grn. 4.343
JJ-G9(1969)
20-011-App. Tr. 4.343

1207+22.4 (JJ)
182+20.0 (7)

COUNTY

RICHLAND
POP. 1783
IN LACLEDE CO. 42
IN PULASKI CO. 1733
IN CAMDEN CO. 8

TO WAYNESVILLE
TO RTE 17

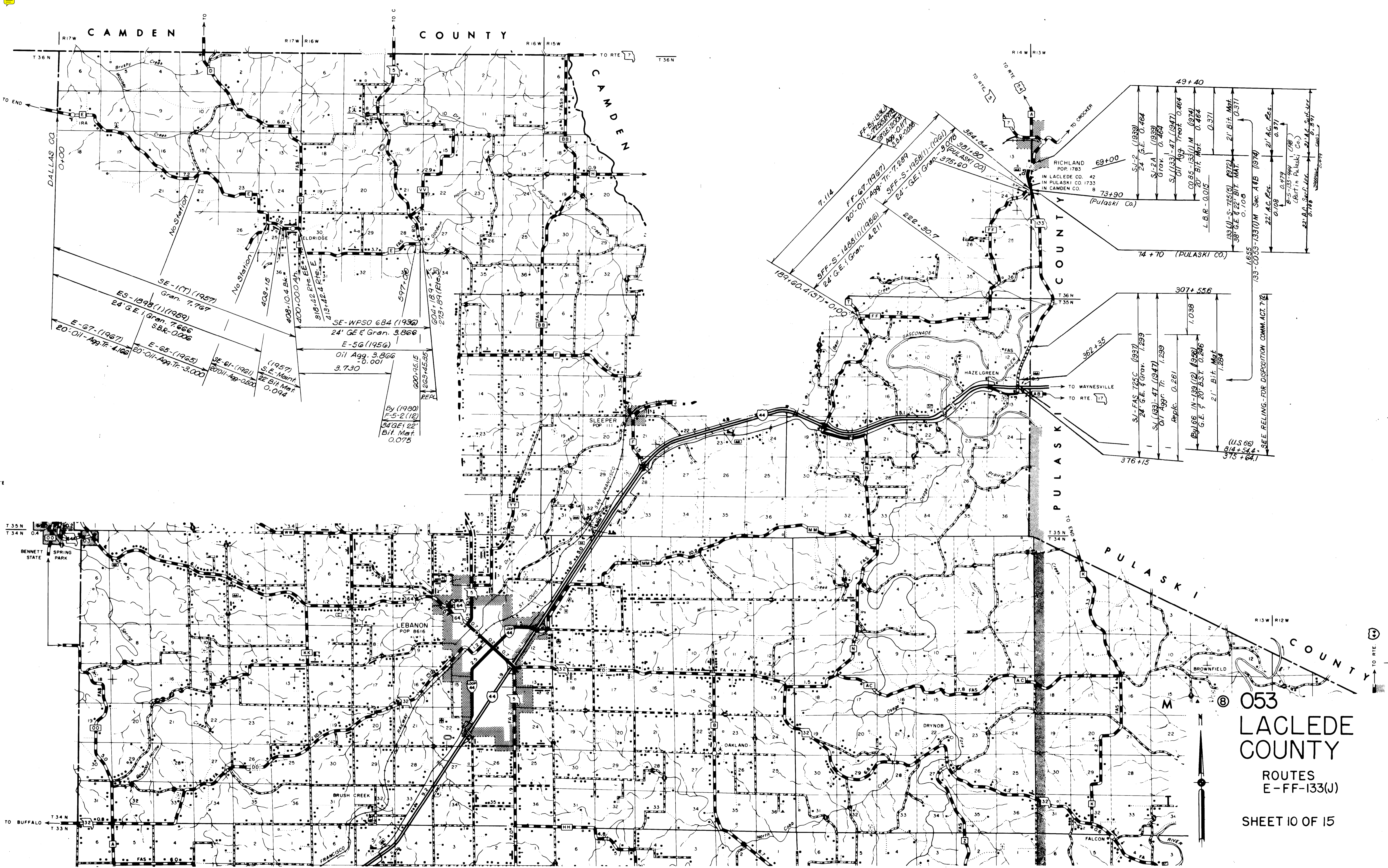
TO END
TO RTE 17

PULASKI

053
LACLEDE
COUNTY

ROUTES
D-AA-JJ

SHEET 9 OF 15



CAMDEN COUNTY

PULASKI COUNTY

053
LACLEDE COUNTY
ROUTES
E-FF-133(J)

SHEET 10 OF 15

DALLAS CO. 0+00
TO END

ES-1898(1)(1957)
Gran. 7,767
24' G.E. 1

E-67-(1967)
20-01-App.Tr. 1,106

E-65-(1965)
20-01-App.Tr. 3,000

SE-61-(1961)
20-01-App. 0.000

(1957)
S.E. Maint.
22' Bit. Mat. 0.094

SE-1(7)(1957)

SE-WFSO 684 (1930)
24' GE & Gran. 3,866

E-56(1956)
Oil Agg. 3,866
-0.001
3,730

By (1980)
F-5-2(12)
34' GE 1 22'
Bit. Mat. 0.075

409+15
408+10.4 Bk.
200+00.0 A.H.
919+12 R.H. E.E.
413+42.4 R.H. E.
597+00
203+45.56
203+45.56
REFL.

7.114
FF-B-1594
S. 7235177
1892-2001
M.C.H.M.
S.P. 2-2008

FF-67(1907)
20-01-App.Tr. 7,289

SFF-S-1488(1)(1956)
24' G.E. 1 Gran. 4,211

SFF-S-1958(1)(1961)
24' G.E. 1 Gran. 3,756.60

9.070
531+80
PULASKI CO.

354+81.7

69+00
73+90
(Pulaski Co.)

SI-2 (1939)
24' G.E. 0.464

SI-2A (1939)
Grav. 0.464

SI(133)-47 (1947)
Oil Agg. Treat. 0.464

CO.85-133(J)M (1974)
20' Bit. Mat. 0.464

L.B.R.-0.015
0.371

133(J)-S-765(G) (1972)
21' Bit. Mat. 0.371

38' G.E. & 22' BIT. MAT.
0.108

1655
133-CO553-133(J)M Sec. A 4B (1974)
22' A.C. Res. 0.371
0.108
0.479
E.S-735-444 (198)
Start in Pulaski Co.)
22' A.C. Surf. Lev. 0.391
0.108

74+70 (PULASKI CO.)

307+53.6

362+35

SI-FAS-725C (1937)
24' G.E. & Grav. 1,299

SI(133)-47 (1947)
Oil Agg. Tr. 1,299

Replc. 0.261

1,038

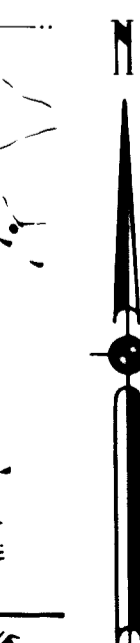
EWJ 66 IN-133(12) (1956)
G.E. & 20' B.S.T. 0.246

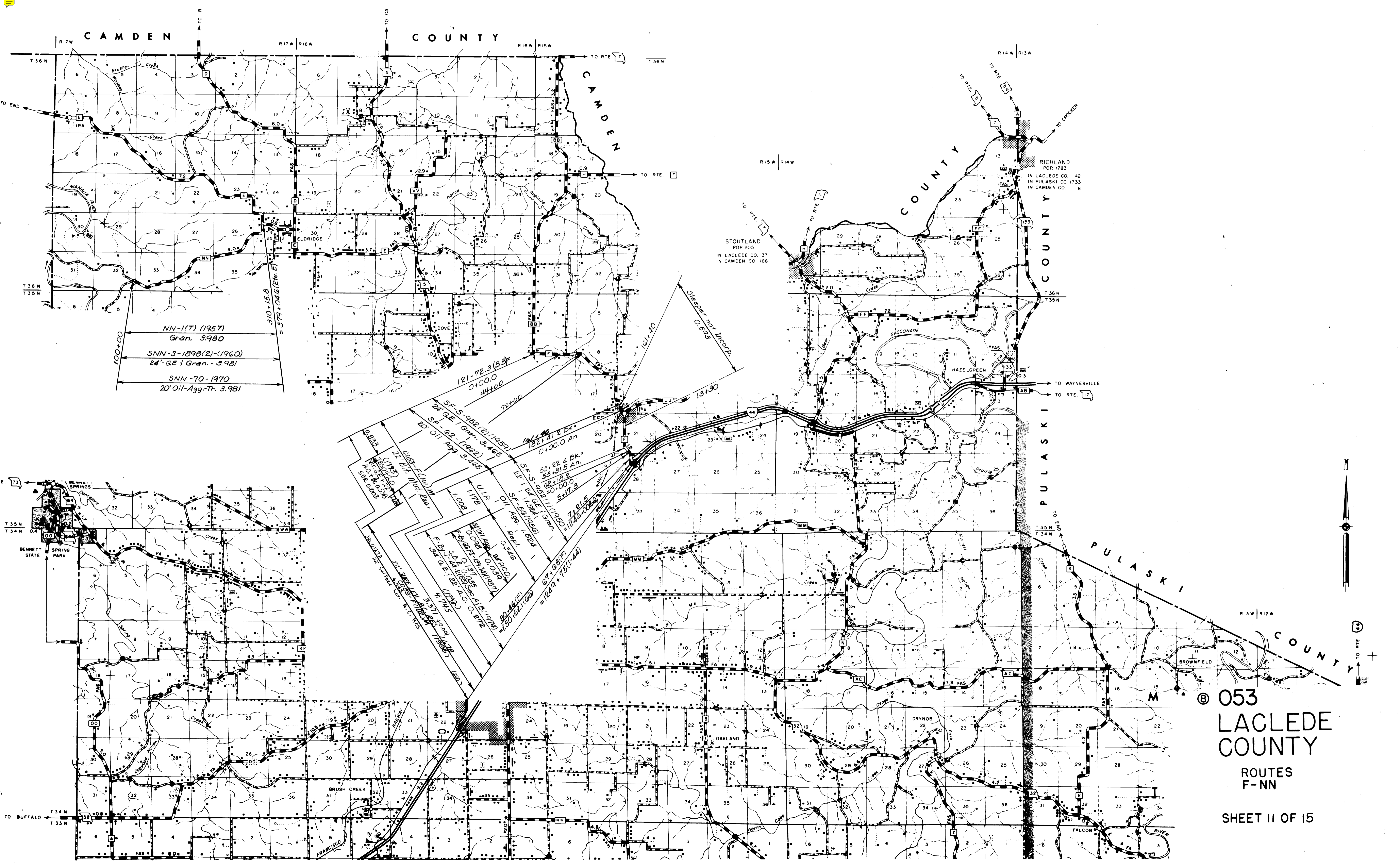
21' Bit. Mat. 1.284

(U.S. 66)
914+54.4
375+64.1

SEE RELING-FOR DISPOSITION COMM. ACT. 7-2

TO RTE. 17





100+00
 NN-1(T) (1957)
 Gran. 3.980
 SNN-S-1898(2)-(1960)
 24'-GE's Gran.-3.981
 SNN-70-1970
 20' Oil-Agg-Tr. 3.981

310+15.8
 = 379+04.6 (E) (E)

121+72.3 (BB)
 0+00.0
 44+00

161+40
 182+41.2 BK
 0+00.0 Ah

SF-S-952(2)(1952)
 24'-GE's Gran. 3.465
 SF-62-1962(1)
 20' Oil Agg. 3.305

22-011 Mar. Dea.
 (1982)
 155' x 150'
 A.C. 1.530

22-012 Mar. Dea.
 (1982)
 155' x 150'
 A.C. 1.530

U.I.B.
 1.178
 1.008

53+22.4 BK
 53+31.5 Ah
 62+18.2
 20+00.0
 5+17.3
 7+21.5
 = 1246+00.0

SF-S-202(1)(1952)
 22'-GE's Gran. 1.924
 SF-56(1960)
 1.994

24-011 Mar. Dea.
 (1982)
 155' x 150'
 A.C. 1.530

F.B. S. S. A. O. 1973
 24-012 Mar. Dea.
 34'-GE's 22'-A.C. 0.872
 (1982)
 4.1795

24-011 Mar. Dea.
 (1982)
 155' x 150'
 A.C. 1.530

24-012 Mar. Dea.
 (1982)
 155' x 150'
 A.C. 1.530

80+46 (F)
 280+62.1 (G)
 = 124+75 (T-44)

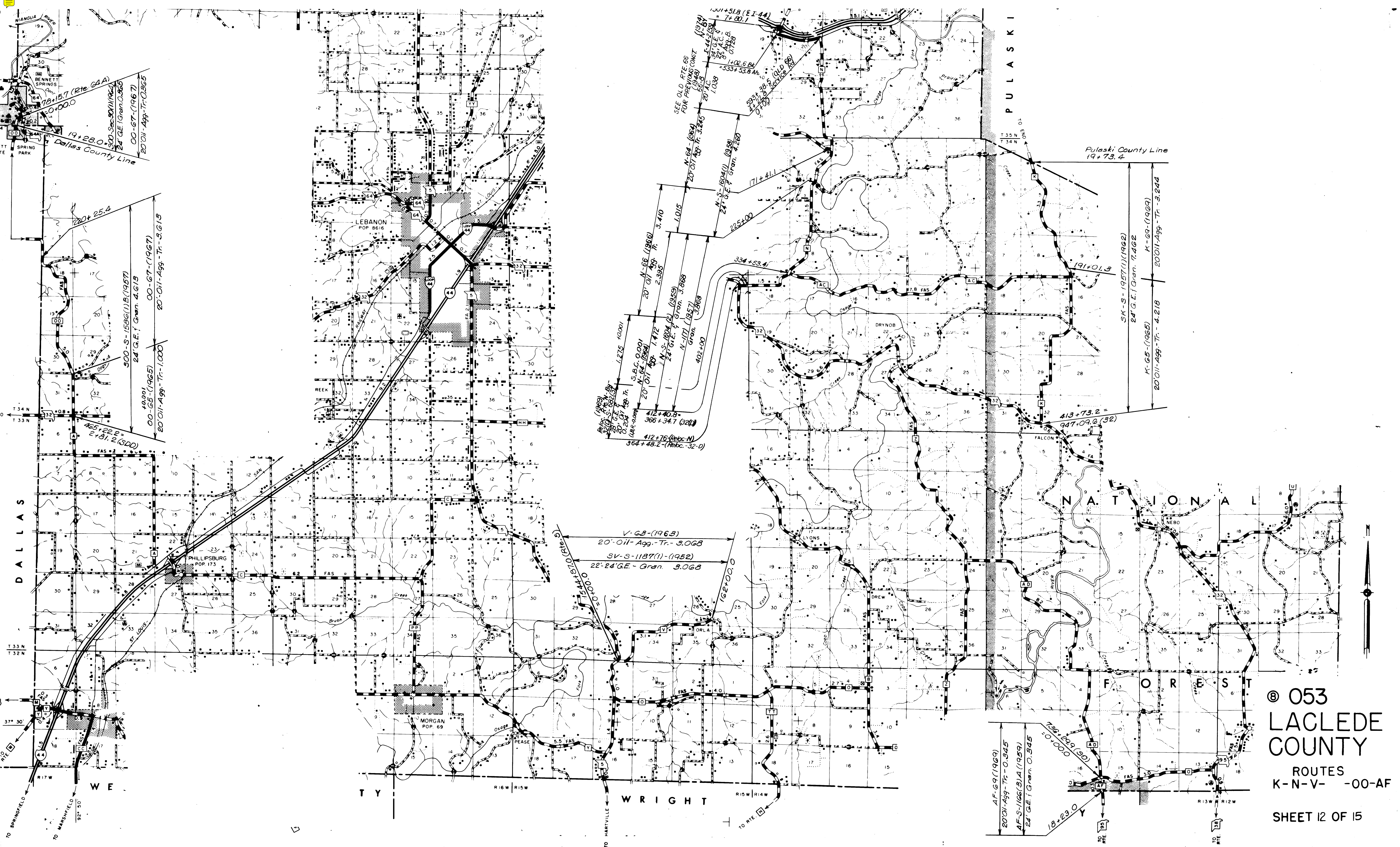
13+30

Sleeper not incorp.
 0.595

RICHLAND
 POP. 1783
 IN LACLEDE CO. 42
 IN PULASKI CO. 1733
 IN CAMDEN CO. 8

STOUTLAND
 POP. 205
 IN LACLEDE CO. 37
 IN CAMDEN CO. 166

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 LACLEDE
 COUNTY
 ROUTES
 F-NN
 SHEET II OF 15



178+57.7 (Rte. 64A)
 19+28.0
 00-67-(1967)
 20' 01" Agg. Tr. - 0.365

220+25.4
 500-S-1586(11.5)(1957)
 24' G.E. Gran. 4.613
 00-65-(1965)
 20' 01" Agg. Tr. - 1.000
 00-67-(1967)
 20' 01" Agg. Tr. - 9.613

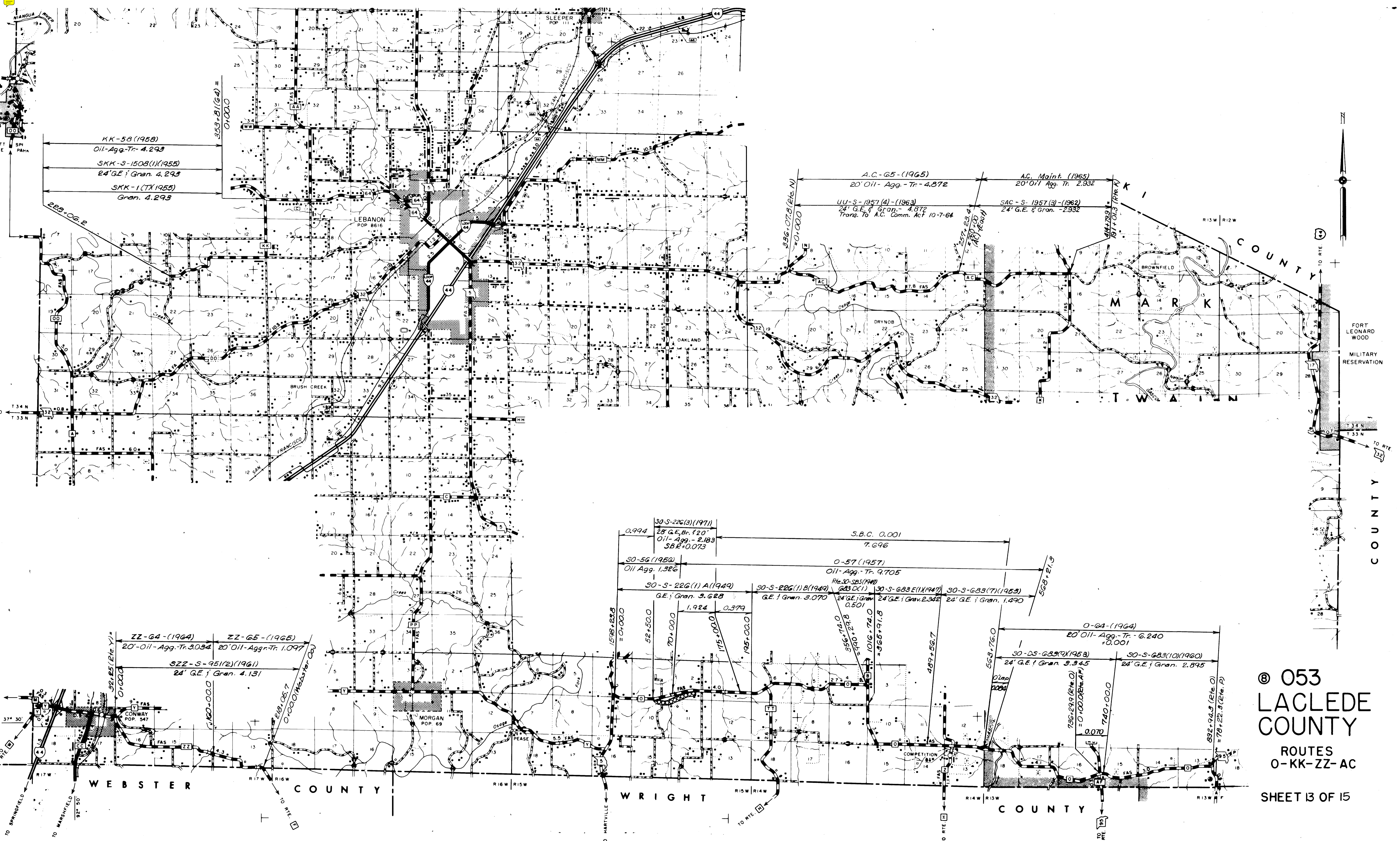
SEE OLD RTE 66 FOR PREVIOUS CONST. (1949)
 N-64-(1964)
 20' 01" Agg. Tr. - 3.245
 1001+51.8 (E.I. 44)
 = 7+80.1
 1402.6 BA
 = 533+53.8 AN
 22' G.E. Gran. 0.728
 20' 01" Agg. Tr. - 1.008
 1001+51.8 (E.I. 44)
 = 7+80.1
 22' G.E. Gran. 0.728
 20' 01" Agg. Tr. - 1.008
 1001+51.8 (E.I. 44)
 = 7+80.1
 22' G.E. Gran. 0.728
 20' 01" Agg. Tr. - 1.008

V-63-(1963)
 20' 01" Agg. Tr. - 3.068
 SV-S-1187(1)-(1952)
 22' 24' G.E. Gran. 3.068

AF-69(1969)
 20' 01" Agg. Tr. - 0.345
 AF-S-1166(3)A(1959)
 24' G.E. Gran. 0.345

Pulaski County Line
 19+73.4
 SK-S-1957(1)(1962)
 24' G.E. Gran. 7.402
 K-65-(1965)
 20' 01" Agg. Tr. - 4.218
 K-69-(1969)
 20' 01" Agg. Tr. - 3.244

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 LACLEDE
 COUNTY
 ROUTES
 K-N-V-00-AF
 SHEET 12 OF 15



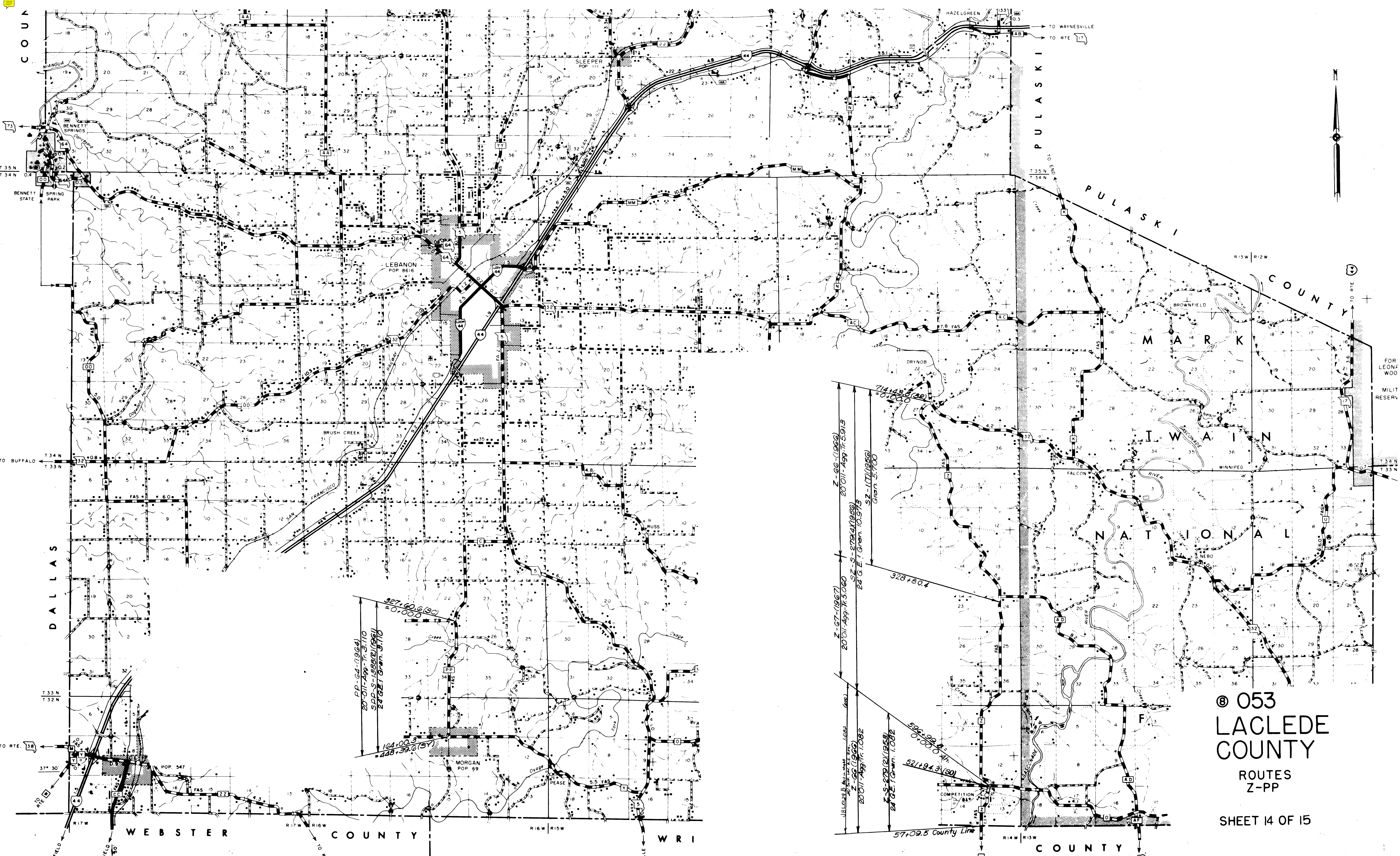
KK-58 (1958)
 Oil-Agg-Tr. 4.293
 SKK-3-1508(1)(1955)
 24' G.E. & Gran. 4.293
 SKK-1(TX)(1955)
 Gran. 4.293

A.C.-65-(1965)
 20' Oil-Agg-Tr. 4.872
 A.C. Maint. (1965)
 20' Oil-Agg-Tr. 2.932
 UU-S-1957(4)-(1963)
 24' G.E. & Gran. 4.872
 Trans. To A.C. Comm. Act 10-7-64
 SAC-S-1957(3)-(1962)
 24' G.E. & Gran. 2.932

50-S-226(3)(1971)
 28' G.E. Br. f 20'
 Oil-Agg. 2.183
 SBR 0.073
 S.B.C. 0.001
 7.696
 0-57 (1957)
 Oil-Agg-Tr. 9.705
 Rte. 30-SBS(1947)
 623 OC(1)
 30-S-683E(1)(1947)
 24' G.E. & Gran. 2.342
 30-S-683(7)(1953)
 24' G.E. & Gran. 1.490
 50-S-226(1)A(1949)
 G.E. & Gran. 3.628
 50-S-226(1)B(1949)
 G.E. & Gran. 3.070
 30-S-683(10)(1960)
 24' G.E. & Gran. 2.895
 0-64-(1964)
 20' Oil-Agg-Tr. 6.240
 +0.001
 50-S-683(9)(1958)
 24' G.E. & Gran. 3.345
 0-070
 740.000
 892.94.3 (Etc. O)
 = 78.22.3 (Etc. D)

ZZ-64-(1964)
 20' Oil-Agg-Tr. 3.034
 ZZ-65-(1965)
 20' Oil-Agg-Tr. 1.097
 522-S-951(2)(1961)
 24' G.E. & Gran. 4.131
 30-S-683 (Etc. Y)
 0.000
 160.00.0
 108.05.7
 0.000 (Revised Co.)

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 LACLEDE
 COUNTY
 ROUTES
 O-KK-ZZ-AC
 SHEET 13 OF 15

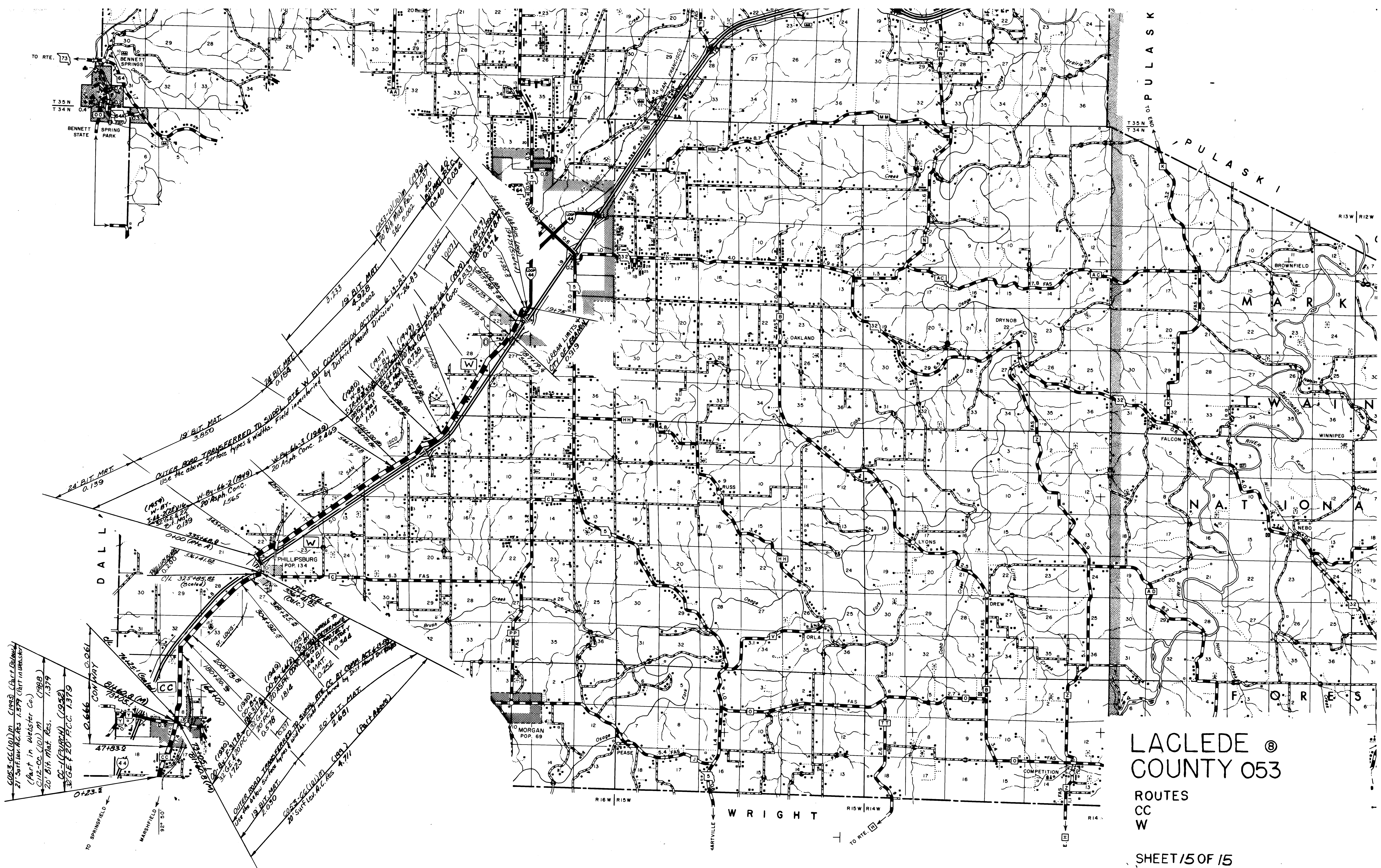


PP-64 (1964)
 20'011-Agg-Tr. 3.710
 SPP-S-1588(1961)
 2476ET Gr-n. 3.710
 164+00.3
 = 248+39.6 (57)

Z-66 (1966)
 20'011-Agg-Tr. 5.060
 SZ-S-2794X (1958)
 24'G.E. Gr-n. 10.973
 SZ-L (1956)
 Gr-n. 5.700
 328+80.4
 Z-67 (1967)
 20'011-Agg-Tr. 5.060
 SZ-S-2794X (1958)
 24'G.E. Gr-n. 10.973
 57+09.5 County Line
 521+94.3 (50)
 536+98.8
 0+000.0 A.P.

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 LACLEDE
 COUNTY
 ROUTES
 Z-PP

SHEET 14 OF 15



LACLEDE COUNTY 053

ROUTES
CC
W